

PLAN
SCALE: 1"=10'-0"

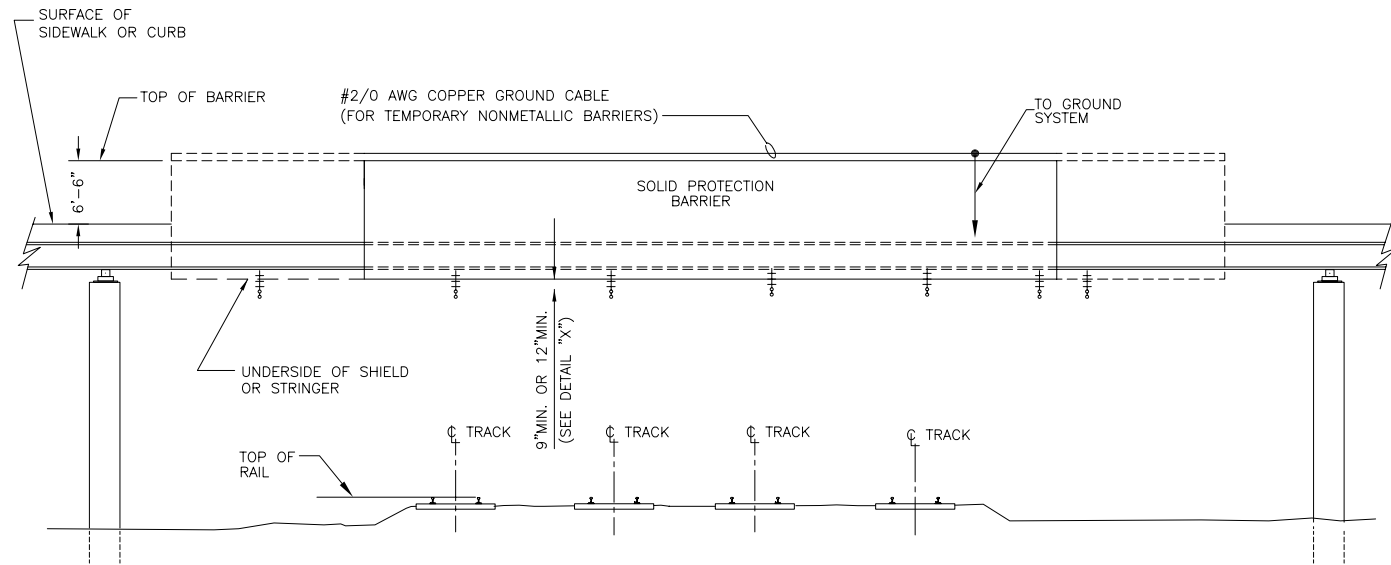
GENERAL NOTES:

- TEMPORARY PROTECTION SHIELDS SHALL BE USED, WITH CERTAIN EXCEPTIONS, DURING DEMOLITION OF EXISTING BRIDGES OR ERECTION OF NEW BRIDGES IN ORDER THAT WORK ON THE BRIDGE STRUCTURE CAN PROCEED OVER THE ELECTRIFICATION FACILITIES WITHOUT REQUIRING DEENERGIZATION OF THE WIRES. ELECTRIFICATION FACILITIES SHALL BE DEENERGIZED DURING THE TIME THE STRUCTURAL FRAME AND THE TEMPORARY PROTECTION SHIELD ARE BEING ERECTED OVER OR NEAR THE WIRES. THE ABOVE WORK SHALL BE DONE UNDER THE DIRECTION OF A QUALIFIED RAILROAD EMPLOYEE.

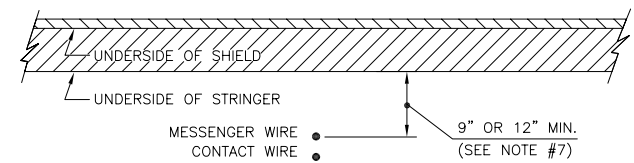
IN CASES WHERE THERE IS INSUFFICIENT ELECTRICAL CLEARANCE BETWEEN THE WIRES AND THE BRIDGE STRUCTURE FOR ERECTION OF A SHIELD, ALL WORK OVER THE WIRES SHALL BE PERFORMED WITH THE WIRES DEENERGIZED AND UNDER THE PROTECTION OF A QUALIFIED RAILROAD EMPLOYEE.

IN CASES WHERE PRESTRESSED BEAMS ARE USED OR WHERE METALLIC FORMS BECOME A PART OF THE PERMANENT BRIDGE STRUCTURE, ERECTION MAY BE ABLE TO PROCEED WITHOUT A SHIELD, IN WHICH CASE ALL WORK OVER THE WIRES DURING ERECTION SHALL BE DONE WITH THE WIRES DEENERGIZED AND UNDER THE PROTECTION OF A QUALIFIED RAILROAD EMPLOYEE.

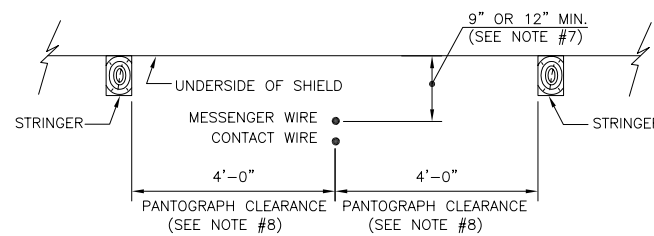
THE TEMPORARY BARRIER SHALL BE INSTALLED WHETHER OR NOT A TEMPORARY SHIELD IS USED.
- DETAILS OF ANY PROPOSED SHIELD AND BARRIER SHALL BE SUBMITTED TO THE RAILROAD FOR APPROVAL, AND WORK ON ANY SHIELD OR BARRIER SHALL NOT BE STARTED BEFORE SUCH APPROVAL IS OBTAINED.
- THE TEMPORARY PROTECTION SHIELDS SHALL BE OF SOLID CONSTRUCTION (TONGUE AND GROOVE OR EQUAL) AND SHALL BE PROVIDED WITH A SOLID PROTECTION BARRIER HAVING A MINIMUM HEIGHT OF 6'-6" ABOVE THE SURFACE OF THE SIDEWALK OR CURB OF THE BRIDGE TO PROTECT WORKMEN AGAINST CONTACT WITH RAILROAD WIRES PASSING UNDER THE BRIDGE AND TO PREVENT DAMAGE TO THE WIRES.
- THE TEMPORARY PROTECTION SHIELD AND BARRIER SHALL EXTEND NOT LESS THAN 10 FEET BEYOND THE OUTERMOST RAILROAD WIRE PASSING UNDER THE BRIDGE MEASURED IN A HORIZONTAL PLANE AND NORMAL TO THE WIRE, AND SHALL PREVENT MATERIALS, AND DEBRIS, FROM FALLING ON OR CONTACTING THE WIRES.
- THE PROTECTION SHIELD SHALL BE DESIGNED FOR A MINIMUM LIVE LOAD OF 100 LBS. PER SQUARE FOOT. IF THE SHIELD IS TO SERVE AS A FORM OR IS TO CARRY ANY PART OF THE OVERHEAD STRUCTURE DURING ERECTION, IT SHALL BE DESIGNED FOR THE SUPERIMPOSED LOADS. IF THE SHIELD IS TO BE USED FOR PROTECTION DURING DEMOLITION OF AN OVERHEAD STRUCTURE, IT SHALL BE DESIGNED FOR A MINIMUM LIVE LOAD OF 100 LBS. PER SQUARE FOOT, OR A CONCENTRATED LIVE LOAD AT ANY POINT OF NOT LESS THAN 2,000 POUNDS.
- NONMETALLIC TEMPORARY PROTECTION BARRIERS SHALL BE PROVIDED WITH 2/0 AWG SIZE COPPER GROUND CABLE CONNECTED TO THE RAILROAD GROUND SYSTEM PER DETAIL "Y", THIS DRAWING. METALLIC BARRIERS SHALL BE BONDED AND GROUNDED BY A METHOD AND WITH MATERIALS APPROVED BY THE ELECTRIC TRACTION DEPARTMENT. THE RAILROAD SHALL INSTALL ALL GROUNDING MATERIALS.
- TEMPORARY PROTECTION SHIELDS OF TIMBER CONSTRUCTION SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 12 INCHES TO THE RAILROAD WIRES. THE CORRESPONDING CLEARANCE TO STEEL CONSTRUCTION SHALL BE 9 INCHES.
- WHERE STRINGERS TRANSVERSE TO THE BRIDGE ARE USED, THE MINIMUM HORIZONTAL CLEARANCE BETWEEN STRINGERS AND RAILROAD WIRES SHALL BE 4 FEET AS SHOWN IN DETAIL "X".
- TEMPORARY PROTECTION BARRIERS SHALL REMAIN IN PLACE AT LEAST UNTIL PERMANENT PROTECTION BARRIERS AND GROUNDING ARE COMPLETED.
- WHERE REQUIRED BY LOCAL CONDITIONS, THE ELECTRICAL CLEARANCES SHOWN ON THIS DRAWING MAY BE INCREASED BY THE ELECTRIC TRACTION DEPARTMENT.
- ANY MODIFICATION OF THE ELECTRICAL REQUIREMENTS SHOWN ON THIS DRAWING SHALL BE SUBMITTED TO THE ELECTRIC TRACTION DEPARTMENT FOR APPROVAL.



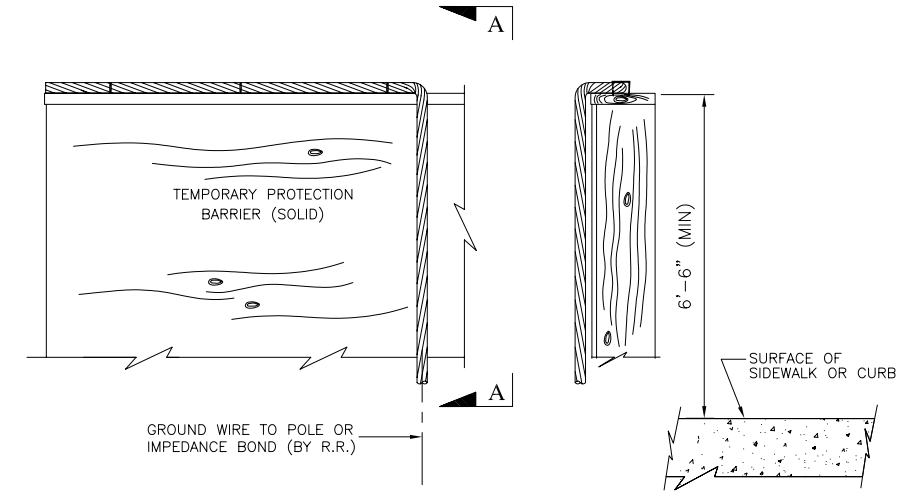
ELEVATION
SCALE: 1"=10'-0"



DETAIL "X"
STRINGERS LONGITUDINAL TO THE BRIDGE
NO SCALE



DETAIL "X"
STRINGERS TRANSVERSE TO THE BRIDGE
NO SCALE



DETAIL "Y"
APPLICATION OF GROUND CABLE
TO TEMPORARY PROTECTION BARRIER
NO SCALE

THIS DRAWING SUPERSEDES P.R.R. DRAWING ET-1447-D-2

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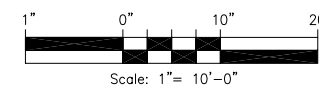
No	Revisions	Date	By



OFFICE OF
V.P., Chief Engineer
Engineering

National Railroad Passenger Corporation
30TH Street Station-Philadelphia, Pennsylvania 19104

Approved	Date
Chief Engineer Electric Traction - R. J. Verhelle	1/18/00
/S/	
Director Electric Traction Design - M. D. Insogna	1/18/00
/S/	



E. T. STANDARD
ELECTRIFIED TERRITORY O.H. BRIDGES
TEMPORARY PROTECTION SHIELD & BARRIERS

Designed: DPT Drawn: BJT Checked: MDI Date: 01-13-00

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Sheet No.: 1 of 1
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